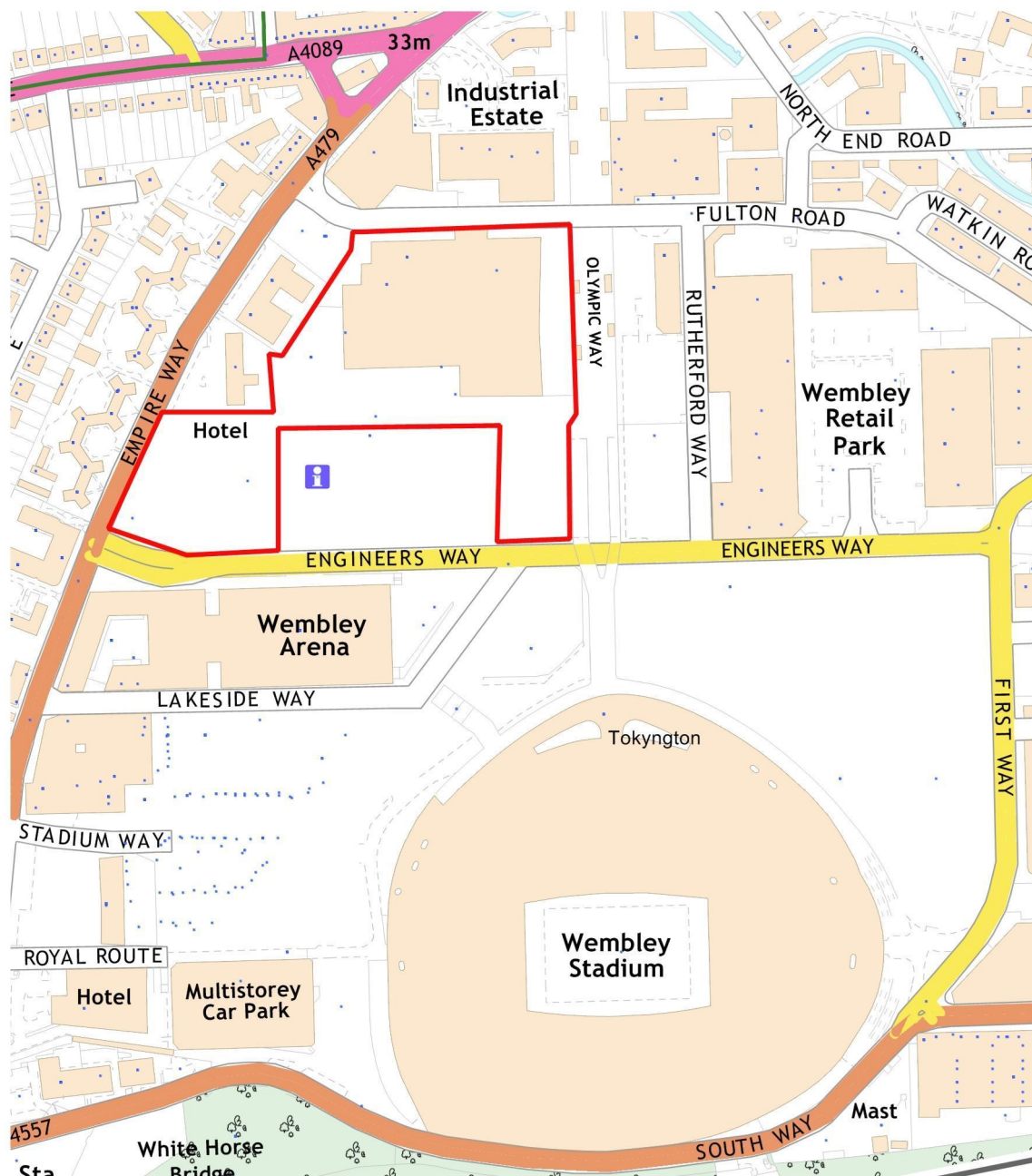




Planning Committee Map

Site address: Former Palace of Arts & Palace of Industry Site, Engineers Way, Wembley

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This map is indicative only.

RECEIVED: 19 December, 2012

WARD: Tokyngton

PLANNING AREA: Wembley Consultative Forum

LOCATION: Former Palace of Arts & Palace of Industry Site, Engineers Way, Wembley

PROPOSAL: The use of the site for surface car parking for up to 1,350 cars for a temporary period of 3 years while land to the south of Engineers Way is redeveloped pursuant to planning permission 03/3200 and the making good of part of the site and other minor works following the demolition of the former Palace of Industry building. The application site is situated between Engineers Way, Olympic Way, Fulton Road and Empire Way but excludes the Quality Hotel, Dexion and Howarine House, the Civic Centre and Malcolm/Fulton House sites.

APPLICANT: Quintain Estates and Development PLC

CONTACT: Signet Planning Ltd

PLAN NO'S:
Please see condition 2.

RECOMMENDATION

That full planning permission is granted for a period of 3 years from the commencement of the use, subject to the ability to agree an extension to this period of time through condition.

EXISTING

The subject site is situated between Engineers Way, Empire Way, Fulton Road and Olympic Way but excludes the Civic Centre site, the Quality Hotel, Dexion House and the site formerly occupied by Malcolm and Fulton House. These named sites and building adjoin the subject site.

The southern parts of the site were cleared some time ago and have been used for various temporary purposes, including the Badminton warm-up facility for the London 2012 Olympics. The northern part of the site is occupied by the "Palace of Industry Building" which formed a part of the Empire Exhibition. This building was Listed until 2004 when it was removed from the statutory list by English Heritage. It is unoccupied and is to be demolished imminently.

The entire application site is the subject of an outline planning consent which was granted in 2011 for the comprehensive redevelopment of the site to provide 160,000 square metres of mixed use development, including residential, retail, hotel, office, student accommodation, workspace and other uses.

The site benefits from an existing consent for temporary use for parking for up to 1,300 vehicles which was granted in 2006. This consent also included the land that is occupied by the Civic Centre. The use of the land for parking

The site adjoins the Quintain "Stage 1 consent" site which is currently being built out (see Site History for more detail). The "Stage 1 consent" site included the Wembley Arena, a grade II listed building previously known as the Empire Pool which is adjacent to this site, on the opposite site of Engineers Way.

DEVELOPMENT SCHEDULE

The table(s) below indicate the existing and proposed uses at the site and their respective floorspace and a breakdown of any dwellings proposed at the site.

Floorspace Breakdown

USE

Number	Primary Use	Sub Use
1	sui generis	private car parks

FLOORSPACE in sqm

Number	Existing	Retained	Lost	New	Net gain
1					

TOTALS in sqm

Totals	Existing	Retained	Lost	New	Net gain

PROPOSAL

This application seeks consent for the use of the land for parking for a temporary period to provide parking that would have otherwise been provided on the Quintain "Stage 1 Consent" site. This change of use would allow parking to continue to be provided while the various buildings within the "Stage 1 Consent" site are built out and thus allows Quintain to provide the car and/or coach parking spaces that it is legally obliged to provide for Wembley Stadium Events. The total number of parking spaces would not exceed the level permitted within the Stage 1 consent.

For example, the demolition of the majority of the multi-storey car park is due to commence later this year which will displace 1,200 parking spaces until the new permanent multi-storey car park and associated temporary spaces on Plot W10 are provided. This results in a significant shortfall in parking spaces during the intervening period which must be addressed in the immediate vicinity of the Stadium. The temporary car park that is the subject of this application would address that requirement.

HISTORY

Ref 10/3032 – The Quintain "North West Lands consent", granted 24 November 2011

This 15 years planning consent has not been commenced yet, but is likely to commence in 2014.

Outline application, accompanied by an Environmental Impact Assessment, for the demolition of existing buildings and the mixed-use redevelopment of the site to provide up to 160,000m² of floorspace (GEA, excluding infrastructure) comprising:

- *Retail/financial and professional services/food and drink (Use Class A1 to A5): 17,000m² to 30,000m²*
- *Business (Use Class B1): up to 25,000m²;*
- *Hotel (Use Class C1): 5,000m² to 20,000m²;*
- *Residential dwellings (Use Class C3): 65,000m² to 100,000m² (815 to 1,300 units);*
- *Community (Use Class D1): 1,500m² to 3,000m²;*
- *Leisure and Entertainment (Use Class D2): up to 5,000m²;*
- *Student accommodation/serviced apartments/apart-hotels (Sui Generis): 7,500m² to 25,000m²;*

and associated infrastructure including footways, roads, parking, cycle parking, servicing, open spaces, landscaping, plant, utilities and works to Olympic Way, and subject to a Deed of Agreement dated 24 November 2011 under Section 106 of the Town and Country Planning Act 1990, as amended

05/2353 – The 2005 consent for temporary parking on this site, granted 27 April 2006

Use of the land for temporary parking for a period of three years from demolition of the western multi-storey car park associated with the Stage 1 Development Area, the new Wembley Stadium, Wembley Arena and Wembley Sunday Market

Associated consents

The Quintain Stage 1 outline planning consent, reference 03/3200, was granted in September 2004 allowing:

Works for the re-orientation of Wembley Arena

Class A1 (Retail) comprising up to 14,200m² designer retail outlet, 11,800m² sports retailing

Class A1/A2 shops/financial and professional services up to 8,000m² (including up to 2,000m² supermarket)

Class A1 (Retail) comprising up to 400 square metres of hotel boutique retail

Class A3 (Food and Drink), up to 12,700m²

Class B1 (a) (b) and (c) Business, up to 63,000m²

Class C1 (Hotel), up to 25,400m²

Class C1/Sui Generis (Hotel apartments), up to 26,700m²

Class C2 (Residential institutions) up to 5,000m²

Class C3 (dwellings) up to 277,000m², (up to 3,727 dwellings)

Student accommodation (Sui Generis), up to 16,600m²

Class D1 (Non-residential institutions), up to 8,200m²

Class D2 (Assembly and Leisure), up to 28,500m² (including the existing Arena of 13,700m²)

3-storey structure to provide car and coach parking

together with associated open space, public market area (Class A1), hard and soft landscaping, highway and engineering works, electricity sub-station, other utility requirements, other parking and servicing, and improvements to Olympic Way;

Minor non-material amendments to this proposal were approved by the Council in 2006, resulting in revisions to a number of the parameter plans.

Elements of this outline planning consent that have delivered to date includes:

- Works to the re-orient and renovate Wembley Arena
- The construction of Arena Square
- Forum House (Plot W01): 8-storey building arranged around a central courtyard with basement comprising of 286 residential units, 8 composite residential/business units, a retail unit, an employment agency and a crèche
- Quadrant Court (Plot W04): 8-storey building arranged around a central courtyard with basement comprising of 232 residential units, 618m² of local-needs floorspace, 2800m² of designer-outlet retail floorspace, 1469m² of D2 floorspace (gym)
- Plot W05 (Hilton Hotel, Student Accommodation, Food and Drink uses and Designer Outlet, Sports and local needs shopping, although the retail uses have not yet opened)

Plots the have commenced and are under construction

- Reserved Matters consent for Plot W07 has also been approved and works commenced on this plot toward the end of 2011. This plot will comprise a 9-screen multiplex cinema, Food and Drinks floorspace and Designer Outlet retail and is due to open in October.
- Reserved Matters has been granted for a multi-storey car park within Plot W10 and works have commenced on that car park. The majority of the existing multi-storey car park on Plot W10 / the Boulevard is to be demolished this year. However, parts of this car park are to be retained on a temporary basis.

Other development

- Reserved Matters approval has been granted for Plot W03. However, works have not commenced on this scheme and the Powerleague "5-a-side" football centre is currently operating from this plot under at temporary consent.

Various permanent and temporary landscape works have been approved on and surrounding the Boulevard.

POLICY CONSIDERATIONS

NATIONAL

National Planning Policy Framework

REGIONAL

The London Plan 2011

Supplementary Planning Guidance

- Sustainable Design and Construction – Supplementary Planning Guidance (2006)
- Accessible London: achieving an inclusive environment – The London Plan Supplementary Planning Guidance (2004)

LOCAL

Brent Local Development Framework Core Strategy 2010

- CP1 Spatial Development Strategy
- CP5 Placemaking
- CP7 Wembley Growth Area
- CP15 Infrastructure to Support Development

Brent Unitary Development Plan 2004

- BE2 Local Context
- BE3 Urban Structure: Space & Movement
- BE4 Access for disabled people
- BE5 Urban clarity and safety
- BE6 Landscape design
- BE7 Streetscene
- BE8 Lighting and light pollution
- BE9 Architectural Quality
- EP3 Local air quality management
- EP6 Contaminated land
- EP12 Flood protection
- TRN1 Transport assessment
- TRN2 Public transport integration
- TRN3 Environmental Impact of Traffic
- TRN4 Measures to make transport impact acceptable
- TRN10 Walkable environments
- TRN11 The London Cycle Network
- TRN12 Road safety and traffic management
- TRN15 Forming an access to a road
- TRN16 The London Road Network
- TRN20 London Distributor Roads
- TRN22 Parking Standards – non-residential developments
- TRN25 Parking in Town Centres
- TRN28 Restrictions on Off-Street Public Parking and Contract Parking
- TRN31 Design and Land Take of Car Parks
- TRN34 Servicing in new developments
- TRN35 Transport access for disabled people & others with mobility difficulties
- WEM2 Pedestrian Route/Promenade
- WEM7 Access to development – the National Stadium Policy Area
- WEM9 Comprehensive Development – The National Stadium Policy Area
- WEM12 Short stay car parking in the Wembley Regeneration Area
- WEM16 Urban design quality – Wembley Regeneration Area
- WEM17 The public realm – Wembley Regeneration Area

Brent Council Supplementary Planning Guidance and Documents

- SPG3 Forming an access to a road
- SPG12 Access for disabled people
- SPG13 Layout standards for access roads
- SPG17 Design Guide for New Development

Other Council Publications

- Destination Wembley – A framework for development (2003) Supplementary Planning Guidance
- Wembley Masterplan 2009

Wembley Vision (2002)

Wembley From Vision to Reality (2007)

SUSTAINABILITY ASSESSMENT

Not applicable given the type of development proposed and temporary nature of the consent.

CONSULTATION

Statutory (application) consultation process

Consultation letters sent: 21 December 2012

Site notices erected: 10 January 2013

Advertised in local press: 3 January 2013

Third party comments:

Letters of objection were received from 18 people, including representatives of the Wembley History Society, raising the following issues:

- No objection to the use of the site for parking from many objectors, but subject to the following comment regarding the retention of façades;
- The majority of the objections request that the northern and eastern façades of the building should be retained for a further two years so that they are in place for the 90th anniversary of the Empire Exhibition;
- Some comments set out a strong objection to the demolition of the building (as opposed to the comments which request the retention of the two façades referred to above. They highlight the importance of the building, being the last remaining structure from the 1924/25 British Empire Exhibition, designed by Maxwell Ayrton and Owen Williams.
- Two comments object to the environmental impact of the provision of parking, setting out that the car park does not correspond to Brent Council's supposed green credentials. Reference is made to the Willesden Green Library Centre application, setting out that the car parking provided within the Palace of Arts and Industry application indicates that the Willesden Green Library Centre application is inconsistent with the requirements of the local community with regard to parking.
- Concern is raised regarding flooding and water quality given that this area used to be prone to extensive flooding due to poor drainage through the Middlesex clay. The objector recommends that substantially better drainage and water cleaning facilities are installed to protect the local water table and drinking water given that this site is to be used as a car park.

The following is officer commentary on these objections

Retention of façade/building.

The majority of the objections request the retention of the eastern and northern façade of the building for a period of 2 years. The Wembley History Society also contacted the applicant and agent directly regarding this matter. The applicant has specified that they cannot feasibly retain those façades for structural (and thus, safety) reasons and highlight the fact that the regeneration of the site and area will deliver a significant number of jobs within the locality. Some objectors would like to see the building itself retained, rather than just the façade.

The demolition of the building does not require further planning consent as the building is no longer listed. English Heritage lifted that designation approximately 9 years ago and the building no longer benefits from any additional forms of protection. As such, the demolition of the building is not the subject of this application and this matter should not form a part of the consideration of this application.

Quintain have specified that demolition is likely to commence prior to the Planning Committee meeting to ensure that the car park is available for use when the existing multi-storey car park adjacent to the Holiday Inn is demolished.

Environmental Impact of this level of parking for the Council

This is not a Council proposal. This application has been submitted by Quintain and it relates to the provision of parking spaces that have been approved through their "Stage 1 Consent". This included parking for

specific uses together with general “town centre” spaces and parking for Stadium events, with “town centre” spaces used for Stadium events on event days. As such, a condition will be attached to ensure that the total level of non-residential parking provided within the application site and the “Stage 1 consent” site does not exceed those levels permissible within the “Stage 1 consent”. The proposal therefore does not result in additional vehicles travelling to the area, but rather results in comparatively minor changes to the precise location at which parking occurs.

Flooding and Water Quality

This application is referable to the Environment Agency and they have accordingly considered such matters. Their comments are discussed later in this report.

Internal Consultees

Transportation:

Transportation have informally commented that they do not object in principle to the use of the land for the proposed purposes but require further controls (to those proposed) to be secured through the Parking Management Plan that is required to be revised and approved subject to a condition of this consent and the Section 106 agreement of the “Stage 1 consent”.

Brent Public Safety Team:

No objection

Landscape Design:

Tree planting in pots is recommended at a rate of 1 tree per 10 spaces (135 trees). They suggest instant mix hedging and a verge of ground cover. Permeable surfacing is recommended, and details of plants, materials etc should be secured through condition.

Environmental Health / Safer Streets:

No objection subject to a condition regarding lighting.

Cllr Claudia Hector has also objected to the application, requesting the retention of external walls as discussed above. She specifies that “We will have nothing left of our extraordinary history if characterful, significant structures are needlessly destroyed”.

Sarah Teather MP forwarded the comments from one of her constituents, requesting that her concerns are considered. Her constituent requested that the external walls of the building are retained until the main construction phase is ready.

External Consultees

Thames Water:

Informatives are recommended.

The Environment Agency

The level of surface water storage from the proposed SUDS system (42 % of the 100 year 6 hour storm duration) is considered to be acceptable given the temporary nature of the consent. It is recommended that a small amount of additional storage is provided, however no formal conditions are requested.

London Fire and Emergency Planning Authority

The Brigade is satisfied with the proposals.

Wembley National Stadium Limited / The FA:

WNSL have expressed their support of this application. Comments are provided on traffic routing during events. WNSL note that the covering letter for the application comments generally that event day traffic from this car park and the green car park to the east of the Stadium will be routed east via Great Central Way. WNSL believe that there would be a significant benefits by routing a significant proportion of event day traffic

to the west, via the triangle and Harrow Road to the North Circular Road and via Empire Way, Forty Lane and Blackbird Hill to the North Circular Road. They would welcome discussions with the Council and other stakeholders regarding this issue.

Greater London Authority (the GLA)

This application does not raise any new strategic planning issues [further to the views of the GLA on the previous temporary consent, which was also referable to the Mayor of London. The comments that have been made by TfL are endorsed by the GLA. The GLA does not need to be consulted further on this application.

Transport for London (TfL)

TfL would expect the use of the site to still be controlled by the Traffic Management Plan, to be re-secured via condition. However, it should be noted that the current plan relies heavily on the use of Great Central Way for egress towards the North Circular and it is felt that Harrow Road is underused for egress from the site. TfL considered these issues could be addressed through revisions to the existing Traffic Management Plan in consultation with TfL.

Provision for Blue Badge holders and electric vehicles should be confirmed. A condition is also expected to be imposed on any consent to control the timing of the application and to cap the total car parking numbers across the development site as a whole

REMARKS

General principle of development

When considering this application, your officers consider that the proposed car park, which is to provide for already approved parking spaces that are to be displaced by construction, differs significantly from proposals to provide a new car park that does not serve such a function.

Planning consent for these parking spaces has already been granted in the immediate vicinity through the Quintain "Stage 1 consent". The proposals confirm that the total number of car parking spaces will not exceed the levels approved within that consent and this will be controlled through condition. As such, this application will not consider the need for these parking spaces, but rather will focus on the change in the general principle of the use of the land, the location of the spaces and any associated impacts.

The land that is the subject of this application benefits from Outline Planning permission for a major mixed-use redevelopment scheme. The proposed use would not be seen to be appropriate if it were proposed as a permanent consent as it would constitute the underdevelopment of land for which a significant number of homes and jobs are projected. However, the applicant only seeks consent for a temporary period and the consent would allow the flexibility to deliver development on parts of the site whilst operating the parking arrangements on the remainder of the site. Your officers consider this to be an appropriate period of time subject to a mechanism (condition) which allows the agreement to an extension of this period if required and if considered acceptable.

Turning to the car parking use itself, consent was granted for this use in 2006, with the commencement of that use being triggered by the demolition of the red multi-storey car park (next to the Holiday Inn). Given the need to provide parking for the stadium whilst new developments are constructed, this site is the logical choice for the provision of car parking spaces due to its proximity to the Stadium.

Nevertheless that immediately surrounds the Stadium represents a preferential location for the provision of parking and therefore controls are necessary to ensure that priority is given to the provision of parking within the "Stage 1 consent" area and that this site is only used only used when necessary. This can be secured through the Parking Management Plan which is discussed later in this report. Given this need to prioritise parking on the "Stage 1 consent" site, the Parking Management Plan must include details of the location of parking spaces (i.e. how many spaces within each of the car parking areas) so that these details can be considered and approved.

The adjoining uses comprise office use, food and drink, hotel and the Council's Civic Centre. In some locations there are residential dwellings that are opposite the site (e.g. Forum House and Raglan Court). However, these are separated from the site by a busy road. It is considered that the proposed development will not have an unduly detrimental impact on those uses, particularly given the current state and use of the land.

Your officers consider that the use of the site for car parking as described within the application documents is acceptable in principle subject to conditions as discussed above.

Car Park Layout

The application submission includes an indicative car park layout which is intended to demonstrate that 1,350 cars can be parked on this site. However, the temporary car park would be primarily used for Stadium events and as such, car parking bays would not be marked out on the ground. The car park would be used in a similar fashion to the Green car park (immediately to the east of the Stadium) where cars are directed to the appropriate location by marshals. This approach is often used for events across the country and has been implemented for the Green car park for some time. The requirement for Stadium events is to provide up to 2,900 car parking spaces, or a combination thereof. As such, flexibility is required in the parking layout to accommodate for potential coach parking for Stadium events.

The TfL have queried the provision of Blue Badge parking spaces within the site. Blue badge parking spaces for Stadium events are normally provided in the areas with suitable accessibility as close as possible to the Stadium. At present they are provided in the Green Car Park which immediately adjoins the Stadium. When construction commences on that land then there may be the need to provide the blue badge spaces on the application site. Given the flexible nature of the proposed parking spaces (i.e. not marked out), the location and number of spaces can be provided at the time. This can also be adequately controlled through the Parking Management Plan.

Whilst a flexible arrangement is considered to be acceptable in principle, the arrangement of cars and coaches within the site can have a significant impact on the speed at which the car park can be cleared at the end of a Stadium Event, particularly where there is a combination of cars and coaches. As such, it is considered that the Parking Management Plan should include indicative parking layouts details various combinations of cars and coaches.

TfL have also queried the provision of electric car charging points. They have indicated that there is no specific standard for this type of parking facility and have said that they accept that the temporary and flexible nature of the car park will limit the number of points that is feasible. Quintain have commented that they are providing 80 electric car charging points within the new permanent multi-storey car park within plot W10 ("Stage 1 consent" area) for which construction has recently commenced. There is no requirement to provide electric car charging points within that car park due to the date that Outline consent was granted. As such, this represents additional provision that will provide a greater benefit from the incorporation of charging points within a temporary car park and this is considered to be acceptable.

The indicative drawing shows three access and egress points to the car park. There is also an existing access onto Empire Way and an additional one that is not highlighted as an indicative access point onto Engineers Way. The access and egress points into the site and the routing of traffic can be controlled through the Parking Management Plan which is to be secured through condition and is discussed later in this report.

Routing of Traffic for Stadium Events

At present the majority of car parking capacity (1,700 spaces) is provided in the Green car park to the east of the Stadium. The primary access and egress route is therefore to the east, via Great Central Way to the North Circular Road. The remainder of the parking capacity (1,200 spaces) is provided in the red car park adjacent to the Holiday Inn, with primary access and egress to the west. Wembley National Stadium Limited (WNSL) and Transport for London (TfL) have suggested that more cars could be routed in a westerly direction, via the Triangle and Harrow Road to the North Circular Road or via Empire Way, Forty Lane and Blackbird Hill.

The location of parking spaces in itself directly affects the routing of traffic due to the road closures that are implemented at the end of major stadium events. These closures result in the loss of east-west vehicle movement, thus requiring those parked to the east of the Stadium to leave Wembley via Great Central Way and those parked to the west of the Stadium via Empire Way or Wembley Hill Road.

Decisions regarding the routing of traffic for stadium events are taken by the Council in collaboration with key partners including Transport for London, Wembley National Stadium Limited and the MET Police. Such decisions are captured through the Parking Management Plan which is a Section 106 requirement of the "Stage 1 consent" and is to be secured through condition in relation to this consent. The "Stage 1 consent" requires the approval of a revised "Parking Management Plan" where changes are made to the location of parking which involve more than 500 parking spaces, but excluding ad-hoc (one-off) changes such as those

required by the Police for crowd segregation purposes. Whilst the 500 space limit is likely to capture the first use of the application site, your officers consider that the condition should specify that it must be reviewed prior to first use to ensure that adequate arrangements are in place before the first use even if that includes less than 500 spaces.

The Parking Management Plan is accordingly considered to be an appropriate mechanism to allow the assessment of the access and egress routes associated with the temporary car park and the other car parking facilities within the "Stage 1 consent" area.

Nevertheless, the Council may not necessarily find the routing suggested by the Stadium and TfL to be appropriate for large numbers of cars unless traffic management measures are implemented by TfL on the North Circular Road. The route to the south via the Wembley Triangle is closed upon egress of the Stadium to facilitate the very high pedestrian flows to Wembley Central Station, having strong regard to the use of public transport as the preferred method of access to the Stadium. This therefore results in the routing of traffic to the north along Bridge Road, Forty Lane and Blackbird Hill to the North Circular Road. During the most recent Stadium Event, part of the application site was used for parking (as a "temporary use" of the site) and vehicles were routed in this direction. This caused significant congestion on this route. This highlights the importance of considering routes to and from the stadium within the Parking Management Plan having regard to traffic management measures that may be in place (as implemented by the Council and TfL) and the location of parking, for which the presumption must be to provide parking within the "Stage 1 consent" site unless this is not feasible due to construction works on the Stage 1 plots. Details of routing should demonstrate the routes for traffic as far as the North Circular Road.

Design, Appearance and Lighting (including Landscaping)

A limited suite of changes are proposed to the site given the temporary nature of the consent. This comment excludes the demolition of the Palace of Industry building which is to be demolished outside of this consent and was discussed previously.

The surface of the car park will be "made good", meaning that repairs and levelling will be undertaken where necessary, and some drainage works will be implemented. Light columns (and the associated lighting) will be installed. Details of the columns and lighting have not been provided. However, this can be secured through condition to ensure that the lighting is appropriate with regard to appearance, uniformity of light and light spill.

No soft landscaping has been proposed. However, the applicant has suggested that they intend to undertake some landscaping works in the locality to improve the local environment. The Council's landscape officers have suggested that a large number of trees are planted in pots. However, your officers consider that the cost of planting and maintenance is not reasonable given the temporary nature of the consent and the requirement for the parking to be flexible. Nevertheless, there are opportunities for planting around the periphery of the site and your officers consider it appropriate to require details of landscaping through condition.

Flood Risk Assessment

The site is within Flood Risk Zone 1 (low risk of flooding), however, the site is over 1 hectare in size and as such, this application is accompanied by a Flood Risk Assessment.

The Environment Agency have considered the submitted flood risk assessment which covers both flooding and risk of groundwater contamination.

With regard to the drainage works and measures to minimise the risk of pollutants entering the surface water system, the applicant proposes to puncture the slab and create a series of pits filled with a coarse granular material wrapped in a geo-textile. This will provide on site storage and a degree of cleansing to the surface water run off before seeping into the sub-soil.

A bund will be created around the site using road upstand kerbs to deal with excess water from abnormal storm events.

The Environment Agency have specified that the submitted details are acceptable. They recommend a small amount of additional storage but do not set this out as a requirement. As they consider the level of storage to be acceptable, this application is recommended for refusal on that basis. However, the comments have been passed to the applicant.

Thames Water also do not object to the proposal. They recommend Informatives regarding petrol / oil

interceptors and surface water attenuation.

Contamination

Environmental Health / Safer Streets have commented that remediation measures are not required given the nature of the proposal, where the site is to remain covered by hardsurfacing, possibly with the exception of some small areas of soft landscaping.

It should be noted that remediation measures are required for the redevelopment of the site as consented through the 2011 "North West Lands consent".

Other matters (including the demolition of the existing building)

The matters raised by objectors have been discussed previously within the report.

Whilst discussed previously, it is considered important to reiterate that the demolition of the Palace of Industry Building does not form a part of this proposal as no further permission is required this in relation to Planning.

Conclusion

The proposal seeks to provide parking on a temporary basis to allow the regeneration of the Wembley Area to take place alongside the on-going operation of the Wembley Stadium. The use of the land as a temporary car park (for a period of 3 years with the ability to extend this) is considered to be acceptable in principle and the detailed matters such as lighting, landscaping, access and egress routes, etc can be adequately secured through condition.

Your officers recommend that planning permission is granted.

REASONS FOR CONDITIONS

RECOMMENDATION: Grant Consent

REASON FOR GRANTING

(1) The proposed development is in general accordance with policies contained in the:-

National Planning Policy Framework
London Plan 2011
Brent Local Development Framework Core Strategy 2010
Brent Unitary Development Plan 2004
Council's Supplementary Planning Guidance

CONDITIONS/REASONS:

(1) The use hereby permitted shall be for a temporary period of 3 years commencing on the date that the use commences and the use shall cease at the expiration of the 3 year period unless an alternative expiration date is approved in writing by the Local Planning Authority and the use shall thereafter cease prior to that date.

Reason: The proposal is only acceptable on a temporary basis and in the interests of redevelopment and regeneration plans for Wembley.

(2) The development hereby permitted shall be carried out in accordance with the following approved drawing(s) and/or document(s):

ITL7093-SK-010 Rev E
ITL7093-SK-011 Rev D
ITL7093-SK-013 Rev C
ITL7093-SK-014 Rev B
"Transport Statement" reference SAW/GJ/ITL7093-001E R dated 19 December 2012
"Drainage and floor risk report" reference WEM009 Rev 00 dated 19 December 2012

Reason: For the avoidance of doubt and in the interests of proper planning.

- (3) The temporary parking area hereby approved shall only be used in connection with or in substitution for parking provision displaced as a result of works carried out to implement planning permission 03/3200 and for no other purpose unless otherwise agreed in writing by the Local Planning Authority.

Reason: The proposal is only acceptable on a temporary basis and in the interests of the redevelopment and regeneration plans for Wembley

- (4) The use hereby approved shall be operated in full accordance with the Parking Management Plan, as most recently approved by the Council at the time of operation of the car park, pursuant to Clause 5 of the first schedule of the Deed of Agreement dated 23 May 2012 made under Section 106 and 106A of the Town and Country Planning Act 1990, as amended, relating to planning consent reference 03/3200, and that Parking Management Plan shall:
- Be revised prior to first use of the application site for the purpose hereby approved;
 - Prioritise the provision of parking within the application site for consent reference 03/3200 above parking within the site for this application unless unfeasible for construction reasons;
 - Include details of the location of parking (number of spaces within each parking area);
 - Include indicative stadium event day layouts and operation methodology;
 - Include details of access and egress points and arrangements;
 - Include details of the routing of vehicles on the local network and as far as the North Circular Road;

or as otherwise agreed in writing by the Local Planning Authority.

Reason: In the interest of the free and safe flow of traffic and pedestrians on the highway.

- (5) Notification of the date at which the use hereby approved has commenced shall be submitted to the Local Planning Authority within 10 working days of the commencement of the use.

Reason: The proposal is only acceptable on a temporary basis and in the interests of redevelopment and regeneration plans for Wembley.

- (6) The following details shall be submitted to and approved in writing by the Local Planning Authority prior to use for parking and the development shall be carried out in accordance with the details approved before the parking is operational:
- Details of external lighting, including details of lighting columns and fixtures (including their locations), and details of light levels having regard to safety amenity and sky glow;
 - Details of soft landscaping proposals, including the areas of soft landscaping, plant species, size and density and any hard landscape features;
 - Details of any proposed boundary treatments;
 - Details of any signage;
 - Details of any CCTV fixtures.
 - Details of how the measures detailed within the "Drainage and floor risk report" reference WEM009 Rev 00 dated 19 December 2012 shall be implemented in the site, or other such measures as are submitted to and approved by the Council.

Reason: These details are required to ensure that a satisfactory development is achieved and in the interests of safety, security and amenity.

- (7) The applicant shall maintain a record of the combined total number of non-residential parking spaces that are made available for use for the purpose of parking within the application site relating to planning consent reference 03/3200 and the site for this planning consent (reference 12/3361). The maximum number of parking spaces provided within this application site shall not exceed the lower of:
- 1,350 spaces; and
 - 3,380 spaces minus the total number of spaces that are made available for parking within the application site for planning consent reference 03/3200

or the equivalent, incorporating spaces for coaches and/or mini-buses unless otherwise agreed in writing by the Local Planning Authority.

The record of the number of parking spaces referred to above shall be made available for inspection by the Council upon the Council's reasonable request.

Reason: In the interest of the highway and pedestrian flow and safety.

INFORMATIVES:

- (1) Thames Water would recommend that petrol / oil interceptors be fitted in all car parking / washing / repair facilities. Failure to enforce the effective use of petrol / oil interceptors could result in oil-polluted discharges entering local watercourses.
- (2) With regard to surface water drainage it is the responsibility of a developer to make proper provision for drainage to ground, water courses or a suitable sewer. In respect of surface water it is recommended that the applicant should ensure that storm flows are attenuated or regulated into the receiving public network through on or off site storage. When it is proposed to connect to a combined public sewer, the site drainage should be separate and combined at the final manhole nearest the boundary. Connections are not permitted for the removal of Ground Water. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer Services will be required. They can be contacted on 0845 850 2777. Reason - to ensure that the surface water discharge from the site shall not be detrimental to the existing sewerage system.

REFERENCE DOCUMENTS:

Any person wishing to inspect the above papers should contact David Glover, The Planning Service, Brent House, 349 High Road, Wembley, Middlesex, HA9 6BZ, Tel. No. 020 8937 5344